



Westminster City Council

Paddington Special Policy Area

Public Realm Strategy

June 2003

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Foreword

This document sets out the Public Realm Strategy for the Paddington Special Policy Area (PSPA), now known as Paddington Waterside. The recommendations contained in this document, which has been subject to consultation with amenity groups and developers, will be Supplementary Planning Guidance (SPG) and will also be a design guide for the area for other projects.

The Public Realm Strategy deals with three zones within Paddington Waterside: public areas within development sites, the public highway and the canalside route. Throughout these three areas a contemporary style is proposed for Paddington Waterside, in keeping with the nature of the various developments that are already under construction.

The Strategy recognises the need for development and change but, by specifying materials, furniture and lighting within the open spaces and public highway, seeks to create a particular style and character for the new quarter. The objective is to create and maintain an environment which has the following characteristics:

- simplicity;
- robustness; and
- high quality.

The importance of a rigorous management regime for the public realm is emphasised and the Strategy also seeks to promote the use of materials and equipment that will be easy to service and maintain and provide value and quality through consideration of “whole-life” costing.

The recommendations in this Strategy will provide for improved accessibility for all pedestrians and particular attention is paid to accommodating and assisting people with disabilities.



Paddington Special Policy Area (Paddington Waterside)

1.0 Introduction

This document constitutes Supplementary Planning and Design Guidance and will be utilised in the following ways:

- to guide and co-ordinate Westminster City Council (WCC) Officers;
- to advise and direct WCC consultants and contractors;
- to provide advice and guidance to Paddington Waterside developers, the Paddington Waterside Partnership (PWP) and other organisations involved in Paddington Waterside such as Transport for London (TfL) and the Greater London Authority (GLA); and
- as the basis of an expansion and revision of the appropriate parts of the Westminster City Council Street Furniture Manual.

The Brief

In October 2001, Urban Initiatives were appointed by WCC to prepare a Public Realm Strategy for the Paddington Waterside. The aim of the brief was:

“To develop a coherent strategy for the public realm within Paddington SPA, including paving materials, furniture and lighting, against which development and highway improvement proposals can be judged, to provide appropriate standards of quality, accessibility, legibility and area identification, with seamless links between adopted and private areas.”

Structure of Guidance

This document is structured as follows:

- the background to the strategy is set out in Section 2;
- current and future development proposals, both land use and transportation, within the PSPA are described in Sections 3 and 4;
- the overall public realm strategy is explained in Section 5;
- specific recommendations for the three identified zones within Paddington Waterside are set out in Sections 6-8;
- discussion of pedestrian routes through Paddington Waterside and their availability throughout the various phases of development;
- description of cycling routes through Paddington Waterside;
- information regarding the provision of lavatories in the area; and
- product reference sheets covering all recommended products are provided as appendices.



Praed Street



North Wharf Road



Paddington Central



London Street

2.0 Background

This Supplementary Planning and Design Guidance for the public realm in Paddington Waterside has been developed within the context of a number of existing policies and strategies that have been or are being implemented within the City of Westminster. A brief summary of these is provided below:

City of Westminster Unitary Development Plan

Westminster's Pre-Inquiry Version Second Deposit of the Unitary Development Plan (put on Deposit 26 January 2002) sets the policy context for this SPG.

Of particular relevance to this study is Policy PSPA 6: Ensuring a High Quality Sustainable Environment which states that:

The City Council will seek to ensure that:

- (A) Development proposals take full account of the policies set out in Chapter 9: Environment; with regard to environmental impacts, sustainable design principles and greening of the environment, and where possible promote best practice;
- (B) A pedestrian route, or improvements / extensions to existing access is provided in all canalside developments to achieve an 18 hour (6am to midnight) public access around Paddington Basin;
- (C) Where appropriate, developments fully recognise the recreational, amenity and ecological value of the Canal and incorporate proposals and management arrangements to enhance these aspects;
- (D) Safe and convenient pedestrian and cycle routes through the area, and in particular between the Paddington Waterside and nearby residential areas and public transport, including access for those with disabilities is provided; and
- (E) Public open space or private open space which is available for public use, is provided where possible, in major development schemes. (WCC 2001).

In addition, Policy DES7: Townscape Details states that;

(A) Public art

1. The provision of public art will be encouraged in association with all large development proposals, including redevelopments.
2. Permission will be granted for public art that:
 - 2.1 is of the highest standard of design
 - 2.2 is made of high quality materials
 - 2.3 enhances its setting
 - 2.4 where fixed to a building is integral to the design of the building.

and also

(F) Street Furniture

1. Permission will be granted for street furniture proposals that:
 - 1.1 are of the highest standards of design
 - 1.2 harmonise with the street scene
 - 1.3 are sited so as to minimise visual clutter
 - 1.4 allows people clear and safe passage along the pavement
 - 1.5 in conservation areas, are of appropriate and traditional design.
2. Permission will not be granted for proposals that involve the loss of historic street furniture.

City of Westminster Street Furniture Manual

In 1993 WCC published its Street Furniture Manual. The aim of the document is to encourage well-designed streetscape within the City by the provision of an easily accessible reference document. The document is intended for reference by the

Council staff as well as landowners and developers. The manual includes both general principles of good street furniture design and implementation and detailed product reference sheets.

The principles of the manual still stand and many of the products included are in use within different parts of Westminster. However, it is not proposed to utilise the products in the manual within Paddington Waterside since these are generally heritage/conservation style. Paddington Waterside developments are comprehensive re-build projects and it is not considered appropriate to use the heritage-style Westminster street furniture within a contemporary townscape.

The Street Furniture Manual is currently under review.

Paddington Waterside Partnership Paddington Waterside—Creating a Place

The Paddington Waterside Partnership (formerly the Paddington Regeneration Partnership) published the document "Paddington Waterside—Creating a Place" in August 2001. This is a strategic design guide for the public realm within the boundary of the Paddington Waterside. The introductory material in the document states that:

"In Paddington, there is an opportunity to create a new quarter with a distinct identity, assisted through the careful application of street materials: paving, roads, footpaths, cycleways, lighting, street furniture and signage. This is not to create an environment which is alien to, or different from, the wider character and context of Westminster. It is to help create a recognisable district which promotes the range of conditions that support and sustain developments, ensuring Paddington Waterside will become an exciting and desirable location." (PRP 2001).



"Street Furniture Manual" (WCC)



"Creating a Place" (PRP)

2.0 Background

The document set out some principles for the public realm which are to be adhered to by all members of the Partnership when developing their individual sites. As already indicated, the document is strategic in nature and therefore provides few details in terms of specific streetscene features and materials. However, a number of relevant statements relating the PRP's aspirations for the public realm were made, as follows:

- in reference to street furniture the guidance states that: *"It will have a contemporary and elegant look. The use of light and natural coloured materials such as stainless steel, timber and natural stone together with contemporary designs will contribute to this character."* (PRP 2001);
- surfaces are to be provided in high quality natural materials; and
- a co-ordinated signage strategy is to be produced to ensure consistency within Paddington Waterside

These recommendations have been borne in mind during the development of this Public Realm Strategy.

Paddington Management Zone Environmental Lighting Feasibility Study

The Environmental Lighting Feasibility Study for the Paddington Management Zone was commissioned by WCC and completed in January 2002 (as part of the Single Regeneration Budget 6 programme). The Paddington Management Zone (PMZ) is focused on Praed Street (but also includes roads leading to Praed Street).

The report of the study outlines various options for improvements within the Zone:

Option 1 (Low Cost)—the addition of a limited number of columns in the area to fill gaps and upgrade the overall levels of lighting. New white light sources would be used. This option is not recommended.

Option 2 (Medium Cost)—The introduction of new and replacement designs of street columns of a higher standard of design at key locations (Praed Street). The possibility of using contemporary columns on Praed Street is put forward.

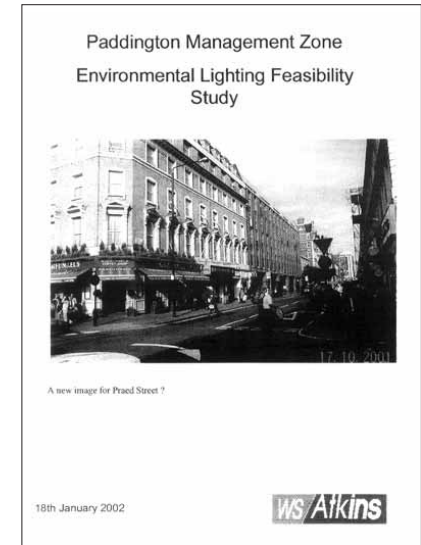
Option 3 (High Cost)—The renewal of all street lights within the PMZ with an agreed contemporary design of columns to co-ordinate with the new developments within the PSPA. White light sources would be used within Praed Street (and possibly throughout the area).

In terms of the design of columns consideration was made of Westminster's Millennium range but at the time of writing no details of the range were available. Therefore use of the Woodhouse contemporary range "Geo" is put forward as a possible option for use in the PMZ.

City of Westminster Local Direction Signs

This document sets out guidelines for the provision of pedestrian and visitor signs, neighbourhood direction signs and carpark direction signs. The fundamental aim of the guidelines is to minimise street clutter and the proliferation of inessential information. The guidelines were approved by the Planning and Development Committee in November 1992 and revised in 1997.

All of these documents provide the relevant background material for this SPG.



"Environmental Lighting Feasibility Study" (WSA)

3.0 Recent History and Current Development Proposals

The first major development within Paddington Waterside was the Paddington Goods Yard, now called PaddingtonCentral. This commenced in 2000 and the first stage was completed in April 2003.

WCC aim to provide a co-ordinated approach to proposals in order to ensure that the area is developed in a coherent and sustainable way. The table opposite sets out the available streetscape/public realm details for the sites within Paddington Waterside.



Paddington Central Arena

Development	Landscape Details	Status (as at June 2003)
West End Quay	Granite slabs, bush hammer finish, two sizes.	Approved Jan 02. Being implemented
Paddington Basin II/III	Granite slab paving, flame textured and fine picked. York stone.	Details not submitted. Illustrative material to be sent. Blocks D+E revised submission. Details for Phase III submitted.
St. Mary's Hospital	Hardwood timber deck pontoons,	Outline application indicating public realm has been submitted but no details as yet.
Paddington Station—Phase III	Not yet available	Good benchmark standards but not yet submitted in detail.
Paddington Goods Yard (PaddingtonCentral)	Granite cubes, Norwegian Quartzite regular concrete, block paving, resin bonded aggregate, loose gravel, Tegula and York stone.	Outline granted 23 May 2000. Detailed submission. Landscaping approved in principle—approved and submitted in detail.
179 Harrow Road	Granite cubes, Norwegian Quartzite regular concrete, block paving resin bonded aggregate, loose gravel, Tegula and York stone.	Landscaping underway.
Rotunda	Granite cubes, Norwegian Quartzite regular concrete, block paving resin bonded aggregate, loose gravel, Tegula and York stone.	
Triangle Site	Resin bonded aggregate (as temporary surface)	Temporary layout agreed but this is a temporary situation, pending Paddington Station redevelopment Phase III.
Hermitage Street Site	Limited space in development. Granite kerb, granite setts on parking bays and speed feature. Dark granite slabs in pool feature.	Outline permission granted. Detailed application approved.
Hilton London Metropole	Modular paving and bollards installed on Harbert Road (private).	Project completed.
Hilton London Paddington	York stone on public footway. Pink natural granite cubes on private frontage.	Project completed.
Canalside	Granite fantail pattern setts	Installed in much of Paddington Basin and in PaddingtonCentral.

3.0 Recent History and Current Development Proposals

Paddington Area Transport Study/Paddington Area Traffic and Environmental Study

The Paddington Area Transport Study (PATS), developed on behalf of WCC and London Transport Planning, was published in 1997/98 and developed an overview of the changes in transport activity (public transport, road traffic and pedestrians) within Paddington Waterside as a result of the development proposals. A land use and transportation model was produced to test alternative development scenarios and identify transport capacity constraints. Proposals were developed to maximise public transport usage and cater for the increased pedestrian circulation in the area. Recommendations for the pedestrian environment included:

- reducing the severance from Westway/Harrow Road to movement from the north;
- the need for a review of crossing opportunities in Praed Street and environmental treatments generally;
- providing access to the canal and utilising it as a core corridor to move about the development lands;
- co-ordinated signing and legibility improvements throughout the area; and
- potential review of pedestrian crossing in Eastbourne Terrace to improve crossing opportunities and give greater focus to the Clock Arch entrance.

In order to ensure that the traffic from new developments does not affect the amenity of the surrounding residential areas the City Council undertook the Paddington Area Traffic and Environmental Management Study (PATEMS). This provides a basis for assessing the impact and the mechanisms for funding measures to manage any traffic

impacts.

Bishop's Bridge Road Improvements

Work is also to be undertaken at Bishop's Bridge Road. The current programme is for Bishop's Bridge Road bridge over the canal and the railway to be closed to pedestrians and vehicular traffic in January 2004, to allow its demolition and subsequent reconstruction, as a single span. Completion is scheduled for late 2005 and the newly finished bridge would provide an opportunity of the introduction of a new range of street furniture and paving materials setting the scene for other major streets within the area.



Bishop's Bridge Road



Praed Street



Porteus Road subway



Edgware Road



Harrow Road subway

4.0 Future Development Proposals

Land Use Development

Proposals are expected for the following sites in the near future:

- Eastbourne Terrace;
- Post Office Site;
- North Westminster Community School;
- Paddington Central II;
- Paddington Central III; and
- Edgware Road at the junction with Harrow Road.

In addition, a planning brief has been adopted for Dudley House. Further briefs will be prepared for:

- North Westminster Community School;
- Travis Perkins, Harrow Road; and
- 55-65 North Wharf Road.

The timescales for development of these sites is as yet uncertain.

Transportation Proposals

CrossRail 1

The CrossRail 1 scheme is designed to provide a new high capacity link from east to west London, serving the City and the West End. The proposed route will include a stop at Paddington station and should decrease the number of interchange movements between underground and Network Rail services at the station. Transport for London and the Strategic Rail Authority (joint developers of the project) are seeking to deposit a Transport and Works Order or Hybrid Bill in late 2003. The earliest opening date will be 2011.

The construction phases of the CrossRail programme will cause considerable disruption within Paddington Waterside and will inevitably require the re-engineering of Eastbourne Terrace. This will provide a further opportunity to introduce a new public space with appropriate street furniture and paving material.

Transport for London Congestion Charging

The Greater London Authority introduced a congestion charging scheme within central London in February 2003. The cordon for the charging zone is the Inner Ring Road which includes Edgware Road (as shown opposite). A network of fixed and mobile cameras inspect the number plates of vehicles entering the charging zone. The necessary street furniture is painted black.

As shown opposite, the zone is close to, but does not abutt Paddington Waterside.



Congestion Charging Zone and Paddington Waterside

5.0 PSPA Public Realm Strategy

Design Principles

This guidance does not seek to define the layout and uses of the major spaces since this is largely beyond the remit of the study and is decided through the planning approvals for the various sites. However, the opportunity is taken to influence the details of landscape materials, street furniture etc. and so assist in delivering the following:

Legibility: People should be able to find their way around the area with ease and to be guided simply and directly to key locations.

Permeability: Physical barriers to movement, and those requiring people to deviate from their natural desire lines, should be minimised or eliminated.

Safety: People should feel safe when they walk or cycle around the area, both in relation to the risk of personal attack and to road safety hazards.

Priority: Conflicts between pedestrians and cyclists should be minimised and in certain locations pedestrians and cyclists should have increased priority over vehicular traffic.

Identity: All public realm measures should improve and refine the area's identity, and contribute to an enhanced sense of place.

Co-ordination: This is essential at two levels: between the agencies responsible for designing, implementing and subsequently maintaining the streetscene; and, functionally and aesthetically, between all the different elements of the public realm itself.

Maintenance: The physical design and management of the public realm should be such as to ensure that it is kept in good working order and visually attractive.

Quality: The design and elements of the public realm should be of a standard in keeping with the surrounding urban fabric.

Accessibility: The design of the public realm should permit accessibility to the PSPA for all members of the community including those with disabilities, the elderly and children.

Sustainability: An approach is required that will ensure the provision of materials and equipment that will remain in operation, in good condition and remain visually attractive with only planned maintenance requirements for a reasonable length of time and not become outdated or worn out too quickly.

The recommendations made in the following sections will seek to apply these principles.

Key Issues

Following on from the generic principles, set out above, a number of key issues specific to Paddington Waterside have emerged in developing the overall strategy. It should be noted that at the time that the brief was commissioned, several developers of sites within Paddington Waterside had already obtained detailed planning approval for their proposals. The details put forward by the various developers for the types of surfacing materials, furniture and lighting etc. have been taken into account when preparing this Strategy, to avoid the possibility of those well-advanced developments being different from others.

Appropriate quality

Many of the sites within Paddington Waterside are being developed as a result of a predicted favourable return on investment. This Public Realm Strategy must strike a balance between specifying standards of materials and treatments of disproportionately high cost whilst upholding and continuing to promote the high standards of urban design and sustainability which are appropriate to the streetscape and spaces within the City of Westminster and to the long term benefit to the tenants, visitors and general public within the Paddington area. These requirements extend particularly to the appropriate choice of materials, longevity of materials and treatments, ease of procurement and maintenance.

The special character of Paddington Waterside

As an area undergoing intense development, the character will inevitably change, reflecting the new uses and associated movements. The construction of new public spaces will also inevitably lead to a radical change to the appearance of the Paddington Waterside. This Public Realm Strategy recognises the need for development and change but, by specifying materials, furniture and lighting within the open spaces and public highway, will help to create a particular style and character for the new quarter. The objective is to create and maintain an environment which has the following characteristics:

- high quality;
- contemporary;
- easy to clean;
- robust;
- safe;
- without impediment to those with disabilities; and
- relates to the surroundings and heritage, particularly the station and canal.

Through pedestrian routes

This Public Realm Strategy includes the requirement that pedestrian routes are provided throughout the area. Historically, this has not been possible due to a range of features including the Paddington Basin, the canals, the railway, the Westway, the nature and use of the development sites and the relative lack of public highways within the area. The obvious opportunity for increasing pedestrian movements is along the canal side and across the canal and to this end various new pedestrian footbridges are being provided.

5.0 PSPA Public Realm Strategy

Hierarchy of Routes

A hierarchy of routes within Paddington Waterside is identified below, defined by the role of each route in terms of pedestrian and vehicular traffic. The routes can be ranked in order of priority as follows:

- Edgware Road—major through-vehicle route, lined by shops and businesses;
- Praed Street—is a primary pedestrian and vehicular route, linking Paddington Waterside to Bayswater to the west and Edgware Road to the east. It also has a role as a local shopping street;
- Bishop's Bridge Road—is a primary vehicular route linking to the A40 and Eastbourne Terrace and an important pedestrian route across the canal and railway lines;
- Canalside—a canalside route has not existed historically but provides the major opportunity to create a key pedestrian network in Paddington Waterside;
- other public highways providing well established routes for both pedestrians and vehicles; and
- routes within the Paddington Waterside developments that provide local links for both pedestrians and vehicles.

This hierarchy has been recognised in the development of the public realm proposals. One of the key principles of the Strategy is to promote connectivity throughout Paddington Waterside and between the area and those surrounding it.

Space Standards

The Strategy requires that care should be taken to ensure that appropriate space standards are provided within the public realm in terms of both settings and use. For example, widths of footways should fit the scale of the urban fabric and also be of the right dimensions to accommodate predicted pedestrian flows. In conjunction with this, street furniture and landscaping should only be introduced at locations that do not affect the safety or amenity of pedestrians and vehicles.

Waste Management Issues

Although recycling facilities are normally restricted to residential developments it is considered that there may be a need for such facilities within Paddington Waterside over time. This subject is not developed further within this document.

Paddington Waterside Public Realm Zones

In order to develop the details of the strategy, Paddington Waterside has been divided into the following zones:

- development sites (public circulation spaces within privately owned space);
- public highway; and
- canalside.

These zones will be covered in Sections 6-8.



Bishop's Bridge Road

6.0 Public Realm in Privately Owned Sites

This zone includes the space between the buildings within the various development sites that are not dedicated as public highways.

General Principles

In line with the aspirations in “Paddington Waterside—Creating a Place”, the routes and spaces within development sites should be designed to a high standard and installed in a co-ordinated way. Materials and equipment should be chosen that can be maintained easily and over a long time period. Developers will be expected to enter into covenants or management agreements regarding the maintenance of their public areas. PWP is co-ordinating a range of management specifications relating to cleansing, security, the waterspace and CCTV and a series of protocols are under development with the aim that all partners sign up at an early stage.

Specific recommendations for materials and equipment to be used are set out below. The Product Reference Sheets (PRS) specified are included in the Appendix.

Paving

A number of the development sites have already had landscaping details approved, and in some cases implemented (see Table 1 Section 3) and as a result it is proposed that the material, agreed for use within these sites is used as the design standard for Paddington Waterside. Therefore light grey natural granite paving slabs are recommended for the remaining sites. The slabs should be cut to precise dimensions and laid to close tolerances to give a crisp contemporary effect. They should also have a textured finish to give good anti-skid properties for pedestrians and cyclists. This can be achieved through flame texturing or a tooled finish on an otherwise regular and even material.

Routes where occasional vehicular use may be necessary should be laid out in smaller size paving slabs, say 300mm or 450mm wide. Other areas inaccessible to vehicles can be laid out in 600mm wide slabs.

See PRS PAn.01

Changes in level within development sites should be marked by corduroy texture pavings.

See PRS PAT.03

Street Furniture

The Geo Street Furniture Range

It is recommended that the Geo range be utilised within the various development site zones.

Currently the Geo range offers an extensive range of equipment relies on proven manufacturing techniques and technology and, from experience gained through specification requirements, can be modified to suit particular purposes without compromising the “essence” of the design.

Specific recommendations for materials and equipment to be used are set out below. The Product Reference Sheets (PRS) specified are included in the Appendix.

Seating

Seating should be provided at appropriate locations within Paddington Waterside. The chosen seating furniture should:

- be designed to prevent skate-boarding;
- use arm rest dividers to prevent use for sleeping;
- have sloping surfaces to allow rainwater to run-off; and
- have timber for the actual seating area.

The Geo Range has a seat that meets the criteria set out above.

See PRS SEs. 02



High-quality detail



Public art emphasising an important entrance



Geo range Park light

6.0 Public Realm in Privately Owned Sites

Litter Bins

Litter bins should be easy to:

- use;
- clean; and
- empty.

Litter bins should ideally be fixed to a pole so that they cannot be moved by unauthorised persons subsequently. A litter bin with a top is recommended to prevent a view of the contents. The Geo litter bin meets all these requirements and is recommended for the private development sites and a bin with a similar shape and form but in black should be used on the public highway .

See PRS BLc.01

Bollards

Normally, bollards are installed as an additional measure to prevent vehicles mounting the footway. All too often the problems are caused at particular locations or in particular circumstances, leading to isolated bollards or short lines of bollards lining the kerb. In many cases, the need for installing these bollards has long since disappeared.

In areas where difficult vehicle manoeuvres occur, resulting in footway over-riding, bollards are often considered an appropriate remedy. However, by the nature of the circumstance the bollards are liable to be hit. This leads to chipping and surface damage, uprooting or deformation, often with consequential damaging effects to the surrounding paved surface.

For these reasons, any proposals to install bollards should be carefully considered. It may be better to suffer the odd vehicle incursion into the footway rather than attempt to prevent it. Where modular paving or the reinforced paving flag recommended

in this document has been correctly laid, it is likely that no paving damage will occur as a result of occasional over-riding. In circumstances where an alternative solution exists, bollards with a highly-visible appearance are suggested; the recommended bollard is shown in the product reference sheet.

See PRS B0m.02

Cycle Stands

The Geo range of street furniture includes cycle stands that will not be damaged by cycles resting against them. A derivation of the "Sheffield" type is recommended as these allow the securing of both wheels and the frame with suitable locks and also provides for the required cycle parking sign within the stand, obviating the need for a separate sign.

See PRS CYm.02

Railings

The use of pedestrian guard railing should be minimised to locations seen as high risk for pedestrians since it contributes to street clutter and requires relatively high levels of maintenance. Styles of pedestrian guard railing developed within Geo range provides particularly good visibility. It is understood that the Council has commissioned consultants to undertake further research regarding use and types of guardrailing in Westminster.

See PRS RAp.02

Lighting Levels and Equipment

Lighting levels generally should be uniform without dark patches, glare or excessive contrast. White light sources are recommended. British Standard 5489 gives guidance on appropriate levels of illumination for all urban locations (an average level of 10 lux and a minimum level of 5 lux) and it is recommended that standards for "minor roads" are set as the appropriate standard of illumination for spaces within the private developments. The recommended lighting equipment is the "Geo" range.

To avoid high surface temperatures on uplighters installed in seating areas, a maximum wattage of 35 must be used for the lighting. New, low heat-generating lamps are being developed and their use should be considered for such locations. Beam angles should be directed at specific subjects for example the face of a café or the underside of a bridge to prevent light pollution.

Any proposals for the lighting of buildings should be relative to the scale of the building. Discrete and defined cold cathode lighting is recommended rather than washes. The range of colours should be limited and it is recommended that blue and white is appropriate for this waterside development.

For health and safety reasons, and to minimise the need for the use of maintenance vehicles, consideration should be given to the provision of demountable street lighting columns. These columns can be lowered by a mechanical device to allow lighting units to be serviced and replaced at ground level. Whilst the hinge mechanism is visually unattractive, attempts are being made by various manufacturers to improve the aesthetic appearance of their products. Examples are produced by the Aluminium Lighting Company, supplier Thorn Lighting, and Abacus.

See PRS LLs.02, LLs.04, LLu.01



Geo range seating



Geo range pedestrian signing

6.0 Public Realm in Privately Owned Sites

Public Art

A study into existing and proposed public art in Paddington Waterside and its immediate area is currently being undertaken by PRP to provide context, encourage co-ordination of public art proposals and highlight opportunities.

Paddington offers many exciting opportunities for new public art. The following points provide key considerations for commissioning:

- Paddington Waterside legibility can be improved by providing landmarks and gateways and highlighting routes through the site. Particular attention could be drawn to buildings, structures and spaces with a public context through the incorporation of art. This could include entrance detailing, lighting and signage;
- The new public open spaces and pedestrian routes in Paddington Waterside provide opportunities for contemporary and innovative sculpture and installations. Such opportunities should be exploited as these elsewhere in the City of Westminster, appropriate spaces are limited in view of its character deriving from a largely historic and tight street pattern;
- The context of a new artwork and its response to place could be provided through its integration with architecture, or its relationship with existing features such as the canal, Westway etc. Integration need not necessarily be physical and could be through less tangible reference and inspiration.
- Public art can help integrate new and adjacent communities in the area and establishing relationships with local art and cultural institutions can facilitate this;
- Public art as well as festivals, events and performances can help to attract and vitalise spaces;

- Temporary as well as permanent works should be commissioned, to give long term interest and encourage innovation. Challenging art is often more acceptable if displayed for a temporary period;
- Public art will be on view during day and night time hours. Appropriate lighting will be required and consideration given to how pieces relate to different light levels, times of day and activity levels;
- Public art could have a major role in the development of Paddington Waterside identity as well as helping to define a marketing image and brand; and
- Interpretation of public art works through name plaques, leaflets and brochures is recommended.
- Great public art is not always created through strict adherence to guidance and each piece will be judged on its merit.

Trees and Other Landscaping

London Plane (*Platanus x hispanica*) is a species widely found around the area adjacent to Paddington Waterside. It is recommended to continue usage within Paddington Waterside where space allows. Other recommended species are as follows:

- *Acer campestre*;
- *Betula pendula* "Dalecarlica";
- *Corylus colurna*;
- *Crataegus monogyna*;
- *Sorbus acuparia*; and
- *Tilia cordata*.

These recommended species are considered appropriate trees within development areas, in groups or as specimen planting, although when specimen planting, many other species could be considered appropriate depending on the context and particular requirements.

Tree surrounds should take the form of a simple tree pit (ideally 1.2m cube), neatly trimmed with a bound aggregate infill of suitable colour appropriate to the surrounding material and include a watering tube.

Landscaping proposals should consider the shape and scale of planted areas and should respect pedestrian desire lines.

A number of principles should be adhered to when choosing plant species, including:

- allowing good visibility and supervision across public areas;
- allowing easy maintenance (small annual growth);
- providing seasonal interest; and
- using native species where possible to encourage wildlife.

Plants that produce fruit should be selected with care to guard against the staining of paving and potential slip hazards.

It is recommended that irrigation systems be built into the landscaping to avoid trailing hoses across footways, creating hazards for pedestrians. Changes of level (using tiers or low walls) can provide added interest to landscaped areas and can act as informal seating.

See PRS TGp.02



Images: Public art in Paddington Waterside

6.0 Public Realm in Privately Owned Sites

CCTV

The prime consideration for the installation of a CCTV system is to manage the public realm, deter crime and to give a feeling of confidence of personal safety to users of the area. Within these parameters, the choice between overt or covert equipment will be established. In urban design terms covert equipment is preferred.

Wherever possible, CCTV cameras should be mounted on buildings and walls to minimise the visual intrusion. Where sight line requirements dictate that cameras need to be installed on poles, they should double-up with lighting or other items of equipment, to minimise clutter.

Conventional lighting columns may not always provide sufficient rigidity, causing loss of picture quality, but this can be overcome with greater wall thickness or reinforcement.

This strategy does not include recommendations for the actual location of cameras but the management of systems should use protocols that are consistent with others that are in the area and those of the Metropolitan Police.

See PRS CTp.01

Pedestrian and Visitor Signing

The Paddington Waterways Partnership (PWP) has commissioned a detailed Signing Strategy for Paddington Waterside that will include work with individual developers.

The Geo Pedestrian signing system has been proposed which has a stainless steel post with extruded aluminium fingers. The background colour of the fingers is blue with white lettering. Further details are provided on the product sheet.

See PRS SLp.02



Geo Highway Lighting Unit

7.0 Public Highway

This section of the guide deals with the areas within the Paddington Waterside that fall within the public highways zone. This zone includes the following routes:

- Bishop's Bridge Road (part);
- Chilworth Street (part);
- Cleveland Terrace (part);
- Eastbourne Terrace;
- Harbet Road (part public highway/part private road);
- London Street (part);
- Norfolk Place (part);
- Praed Street;
- North Wharf Road;
- South Wharf Road;
- Westbourne Bridge (part); and
- Winsland Street.

In addition, Hermitage Street is shortly to be reconstructed as a highway and could be included within this list although outside Paddington Waterside.

General Principles

The Westminster Street Furniture Manual (1993) proposes a range of street furniture products for use on public highways in the City of Westminster. As indicated in Section 2 of this document, the range of options are heritage based, reflecting the historic, architectural and cultural status of the City of Westminster.

However, since Paddington Waterside is being comprehensively redeveloped, it is recommended that the opportunity should be taken to introduce a range of street furniture within the public highway that will enhance the best of the architecture and spaces in the area.

Westminster City Council's standard livery for street furniture is black. The linkage between the private realm and the public highway will

therefore be provided by the use of black street furniture of a similar shapes and styles to those recommended in Section 6, which is based on the 'Geo' range. The street lighting family in particular will provide unity between the zones.

See PRS BLc.01, SEs.02, CYm.02, RAp.02

Paving

Following the establishment of the use of natural granite slab paving within development areas, it is proposed that this effect is continued into the public footway areas. Due to the cost of natural stone, and the areas of footway to be laid, it is suggested that a concrete based paving slab made of granite aggregate is an appropriate, economic solution. This is particularly relevant for large areas of paving. For small, discrete areas natural granite paving may be appropriate. To provide the desired visual impact, concrete slabs should be 600mm wide and either 600mm or 750mm long. Due to the module size and the likely over-riding by vehicles, the slab should be 65mm thick and be reinforced. The combination of a 65mm thick slab, reinforced and laid to the traditional high standards of workmanship employed by Westminster City Council on a concrete base will provide an extremely durable paving system, capable of resisting occasional over-riding for many years. The development of such a slab has been undertaken as a result of this brief, following an approach to Charcon paving. At the time of writing this report they have been supplying the London Borough of Camden with reinforced concrete paving flags for the past two years. From discussion, no obvious defects have appeared to date. However, one consideration is the effect on the reinforcement of cutting the paving slab, to accommodate radiused levels or where public utility inspection covers exist. In Westminster the preferred solution is to use reinforced slabs in large runs where

overrunning is a temptation. Where cutting is required on radiused corners, around inspection covers, or at places where the grid is changed, the cut slabs will be of the same type but un-reinforced. The maintenance liability is considered acceptable in these limited locations. It is proposed that the "standard mix" Charcon natural granite slab is used.

Granite kerbs, 300mm x 200mm should be laid in all footways and as appropriate in private areas to designate the boundary between the footway and the carriageway.

Tactile paving design within Westminster is currently being dealt with on a city-wide basis and recommendations will be made in the forthcoming new edition of the Street Furniture Manual.

Public utility companies should be encouraged to avoid siting junction boxes at road junctions or corners where tactile paving is required.

See PRS PAc.03, KEn.01

Street Furniture Feeder Pillar Posts

24/7 (London Electricity) is experimenting with underground connection boxes to avoid the need for a post. The specification requirements are challenging and it may be some time before an acceptable product is marketed. It is recommended that WCC officers pursue the development of such equipment as a significant contribution to the removal of this item of street clutter. The specification should include:

- ease of entry;
- long-term waterproofing; and
- durability and resistance to deformation when driven over.

Street Nameplates

No change is proposed for street nameplates. It is considered that the existing styles that are well-established and have a timeless quality remain appropriate for Paddington Waterside.

Street Lighting Equipment

WCC commissioned WS Atkins to investigate street lighting in Paddington Management Zone as part of the SRB6 programme (see Section 2). The following recommendations have been made as a result of the work undertaken:

- white light source; and
- contemporary columns (the Geo range).

It is therefore recommended that the Geo range of street lighting is implemented in the remaining areas of Paddington Waterside. The suggested column finish is gloss black to co-ordinate with street furniture within other parts of the City. A coating to resist stickers is recommended for the columns.

Since most of the footways in the area are narrow the use of bracket arms mounted on building facades is an appropriate method of providing street lighting that would minimise obstructions to pedestrians. A structural survey will be required to determine where such bracket mounted lighting units should be installed on the older building stock in Paddington Waterside. In addition, traffic signal equipment should be combined with street lighting equipment where possible.

See PRS LLs.03, LLs.04

CCTV

In the event that CCTV equipment is installed on the public highways, the guidelines should be similar to those for development areas. Exact locations for cameras are not proposed as part of this strategy.

See PRS CTP.01

Pedestrian and Visitor Signing

WCC requirements for pedestrian and visitor signing are set out in the Street Furniture Manual and are described in more detail in ‘Local Direction Signs, Guidelines for the Provision of Pedestrian and Visitor Signs, Neighbourhood Direction Signs and Car Park Signs’. The style of signs and posts are based on heritage principles and a second design on the public highway would not be welcomed at present.

Paddington Waterside as an area will be signed from the public highway using Westminster’s current signage design system.

Trees and Other Landscaping

Tree planting is encouraged at appropriate locations on pavements and in wider paved areas. Trees can help improve the quality of the townscape and strengthen the links between the Paddington Waterside and other parts of the City that are more generously planted. Locations chosen for planting should form groups of trees at strategic locations or correspond with appropriate architectural features on buildings (for example mullions).

The chosen species should be appropriate to the setting and be self-maintaining once established. Uniform treatment should be provided along single streets or within areas. See Section 6 for details of tree surround.

There is limited scope for planting on the public highway as many of the footways are narrow but the principles are those as in Section 6.

The use of hanging baskets would generally be inappropriate in the new spaces of Paddington Waterside. In locations that interface with historic areas that already have hanging basket displays, then at the point of transition they may be considered.

See TGp.02

Local Street Equipment

Other street equipment that is needed by WCC or by Transport for London (TfL) includes parking control, direction signing and traffic signals. The thorough co-ordination of all of these items would provide significant benefit to the visual appearance of the street. At the same time, opportunities to incorporate new technology into equipment should be taken.

Other Street Equipment

It should be recognised that there are many items of furniture (for example: telephone boxes, mail boxes, public utility cabinets) which are installed on the public highway by organisations that do not require the City Council’s express permission and may not be concerned over the appearance, integration or style of their apparatus, and the effect on the streetscene. It is recommended that WCC bring this public realm strategy to the notice of such organisations and request that TfL and the Utility Companies invest in designs that will complement the contemporary style of street furniture.

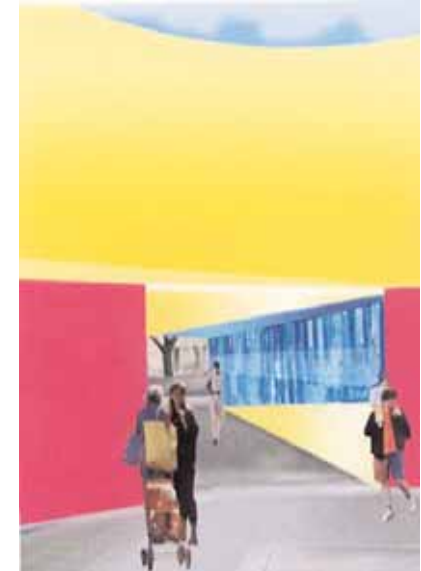
Traffic signal control equipment is the responsibility of Traffic Technology Services (TTS), part of TfL. The equipment that exists at these locations is specified to a London-wide standard and the only local variations are such items as the colour of paint used for signal poles and controllers. From discussion with representatives of Transport Technical Services, it is understood that they would not be averse to research and subsequent introduction of a new range of traffic signal control equipment to match the chosen style of street furniture. Traffic signal controlled junctions exist at the following locations within Paddington Waterside:

- Bishop’s Bridge Road/Eastbourne Terrace;
- Eastbourne Terrace/Praed Street;
- Edgware Road/Praed Street;
- Westbourne Terrace/Westbourne Bridge/Bishop’s Bridge Road; and
- signal control has recently been installed at the London Road/Praed Street junction.

Further studies should be commissioned to research the technical aspects, modern requirements and visual aspirations of a new system so that it could be integrated with the chosen street furniture within Paddington Waterside. Such issues as micro-current technology, reflector design, high intensity illumination and more responsive programming could all be considered.

Westway

The area beside and under the Westway (at Edgware Road) lies outside Paddington Waterside and is the responsibility of TfL. It is seen by the City Council as a key linkage into the area. A scheme associated with Paddington Basin Phase III is being considered by the City Council for improvements. At the subway entrance, by the Police station, proposals include the removal of unnecessary railings, re-paving and the provision of seating.



Porteus Road Subway proposals by Peter Fink



Westway at Edgware Road

8.0 Canalside

General Principles

The overall strategy for the canalside is set out in the PRP document “Paddington Waterside Creating a Place” under the heading Waterspace Strategy (as set out in Section 2). The document states that:

“Given the canal’s central position within the Partnership’s public realm framework, continuity and unity in treatment of the canal and its towpath is of great importance.” (PRP 2001).

It is intended that the canalside will be a key focus for pedestrian activity within Paddington Waterside. The recommendations set out below have been drawn up following discussions with British Waterways London (BWL).

Specific recommendations for materials and equipment to be used are set out below. The Product Reference Sheets (PRS) specified are included in the Appendix.

Bridges

The PRP Waterspace Strategy states that five new pedestrian bridges are to be constructed over the first few years of reconstruction (between the head of the basin at West End Quay and the Pool of Little Venice). The first was opened in 2002 and the majority will be in place by the end of 2003.

Since the architectural style of the bridges will vary considerably, it is recommended that any street furniture required on the bridges (for example lighting) should be chosen to complement the style of the bridge in question.

Paving

The use of granite setts laid to a fantail pattern is already well established along the canalside. This can be seen at the new

developments at West End Quay, Paddington Basin and PaddingtonCentral. It is therefore recommended that this paving material be used for the length of the canalside within Paddington Waterside as its slightly rough finish has the advantage of being a tactile warning of the nearby canal edge. This canalside ‘paving zone’ is appropriate for the first two metres from the water. However, outside this zone the walkways should have the same broad specification but have finer joints and picked surface to assist slip resistance. This will provide improved performance for those with mobility difficulties. Within development sites granite setts should not be used. Instead they should be treated with other sympathetic materials to give smooth surfaces suitable for prams and people with disabilities, including those in wheelchairs (e.g. light grey natural granite paving slabs – see Section 6).

See PRS PAn.01

Lighting

BWL do not wish the canalside to be treated as a public highway in terms of lighting, but rather would prefer an “atmospheric and architectural” approach. The appropriate level of illumination for canalside walkways is defined as a category 3.1 in British Standard 5493: Part 3: 1992 giving an average lux level of 10 and a minimum level of 5. These areas fall into the description “routes where nighttime public use is likely to be high.” This may be associated with amenities such as clubs, shopping facilities, public houses, old people’s homes, etc. or where the crime risk is likely to be high. To denote the canal edge, a domed waterside light set into the canalside is an established precedent for consistency throughout Paddington Waterside. It is recommended that a white light source be used.

See PRS LLs.02, LLs.04

Cycle Facilities

Cycling along the canalside is a contentious issue. BWL has major concerns about the conflicts between pedestrians and cyclists (especially commuter cyclists) on the towpaths. There have already been a number of accidents (including two fatalities) and BWL consider that there are likely to be more if cycling is permitted. However, BWL is aware that it is very difficult to physically prevent cyclists from using the towpath and instead BWL attempt to manage the situation. Cyclists should have permits to use the existing towpath route and a code of conduct for cyclists has been produced by the British Waterways Board. In addition, some design measures have been implemented to slow cyclists down at danger points.

The new connections within Paddington Waterside will provide very attractive (and direct) cycle routes alongside the canal. However, they should not be advertised as cycle routes and measures should be implemented at appropriate locations to slow cyclist’s speeds (e.g. chicanes). Signage should encourage sensible behaviour by cyclists including dismounting and walking at certain locations.

See Section 10 for further information regarding cycle routes.

Lifebelts

BWL currently provide lifebelts at locations that they consider could be dangerous for the public (e.g. operational locks). Within Paddington Waterside there are no locations where lifebelts are required by BWL.

Pedestrian Signs

As indicated in Section 6 a comprehensive Signing Strategy for Paddington Waterside has been commissioned by the PWP.

See PRS SLP.02



The Canal Basin



Granite Setts—Towpath

8.0

Canalside

Surface Water Drainage

It is recommended that even gradients are provided across the canalside footways in order to ensure surface water drainage straight into the canal. This is in order to avoid the need for channels or pits that can cause hazards to pedestrians and do not contribute to the aesthetic qualities of the proposed paving materials.

Trees and Tree Surrounds

Recommendations as in Section 6.

See PRS TGp.02

Railings

Railings are not generally a requirement along canalside towpaths. However, at exceptional locations that are considered dangerous for the public they will be required. These will be limited to narrow canalside routes and areas where pedestrian routes meet the canal towpath at right angles and are utilised by large numbers of pedestrians. The decision to install railings along the canal path will be made by British Waterways. It is recommended that railings from the Geo range are utilised where required.

See PRS RAp.02



Lighting units on the canalside



New canal bridge at Paddington Basin

9.0 Pedestrian Routes

Pedestrian Movement

With Paddington Waterside, pedestrian links, particularly in a north-south direction, are currently poor, due to the existence of the railway, canal and motorway.

The proposed development within the area will affect existing movements and as a result, a part of this study has investigated the effect of development proposals on the ability of pedestrians to move through the area.

Key walking routes that have been identified are:

- between Paddington Station and the area to the north, including Paddington Green and Maida Vale;
- between St Mary's Hospital and the area to the north, again including Paddington Green and Maida Vale;
- north-south access to local shopping areas, Praed Street to the south, Church Street Market to the north-east and Edgware Road to the east;
- employment opportunities created by the new development and the need for commuter walking from Paddington Green and beyond to the Paddington Waterside; and
- the emerging leisure pursuit of walking along the canalside from Little Venice to the head of the basin.

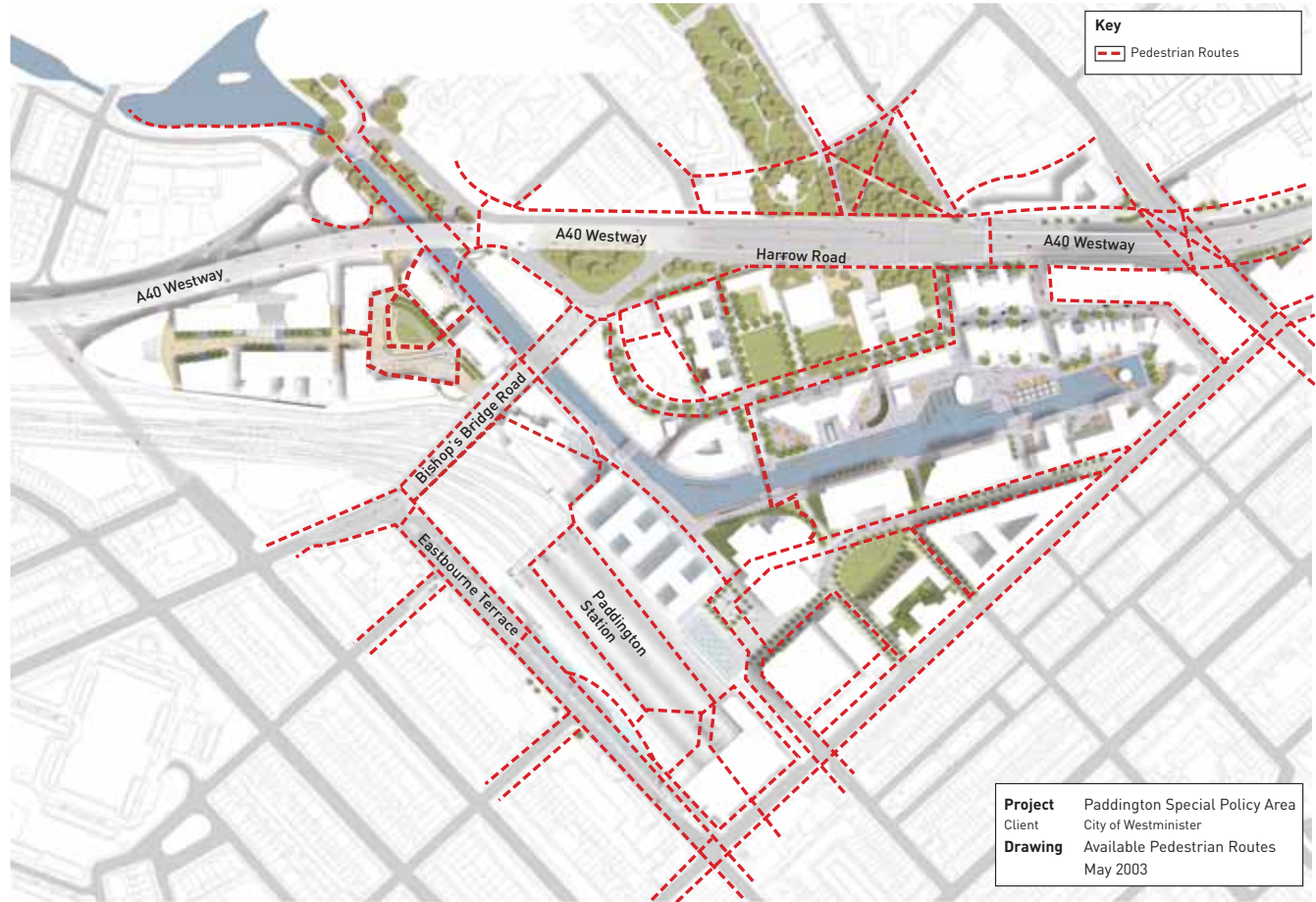
From discussion with developer's representatives, pictures of the layout of available pedestrian routes within the area over each of the next four years have been created on the following pages.

9.0

Pedestrian Routes

By May 2003: the canalside fronting PaddingtonCentral will be open, making it possible to walk from Paddington Station to Little Venice. With the introduction of the canal bridge near to the Porteus Road subway, pedestrian links will be established to the north to Maida Vale.

Sites at Paddington Basin will still be in the course of construction and the redevelopment of St Mary's Hospital will not have commenced, so other areas of the canalside will not be available.



9.0 Pedestrian Routes

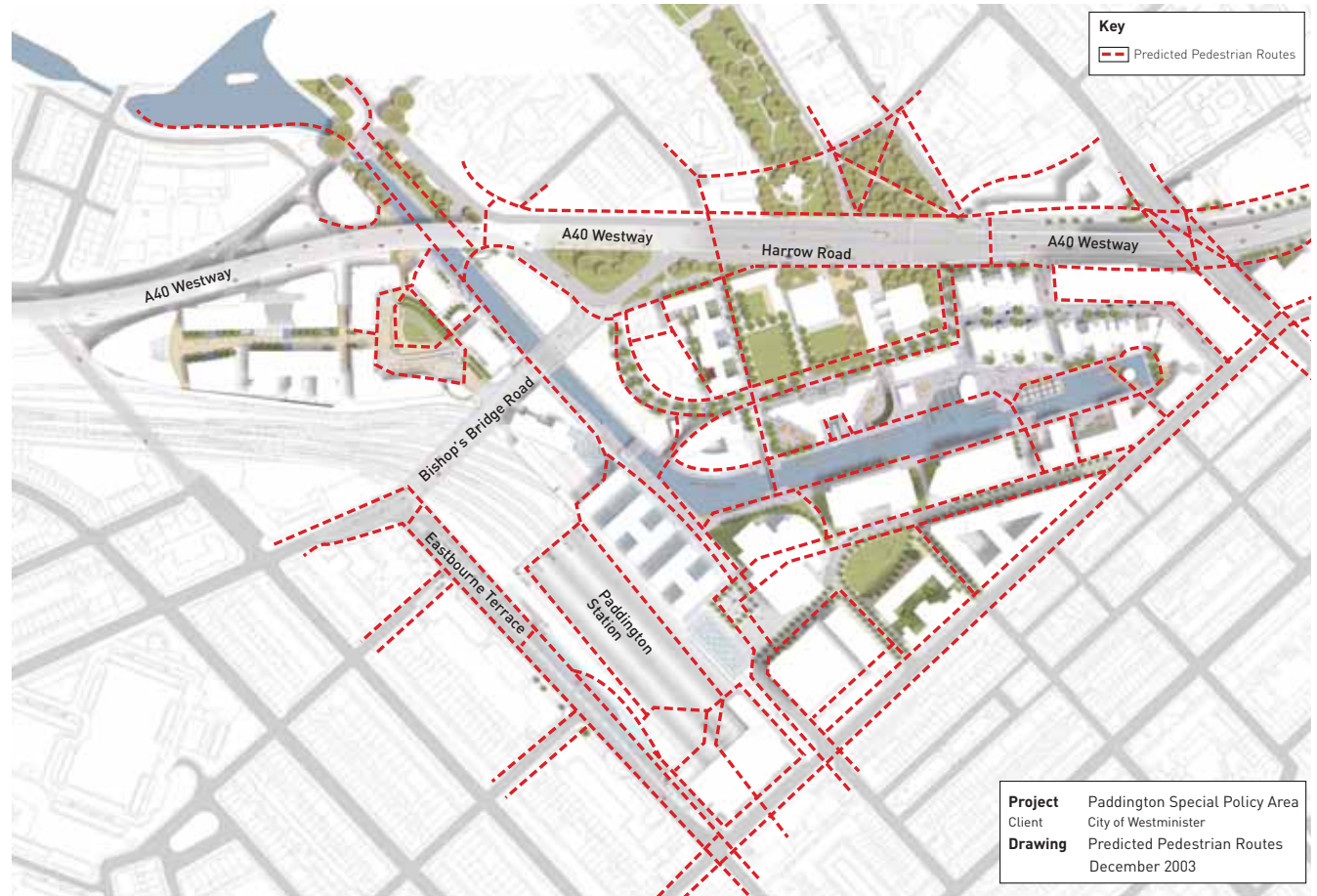
By the end of 2003: the northern side of the basin at the western end will be complete and the eastern end will be temporarily available pending the start of construction on the remaining site.

Whilst St Mary's Hospital development will still not have commenced, the pedestrian bridge across the canal will have a revised access to the south, allowing easier access to South Wharf Road than the present arrangements.

Of major significance to the pedestrian (and traffic) movement within the area is the reconstruction of Bishop's Bridge. This project (part of the Paddington Long Term Vehicular Access Strategy or LTVA) is programmed to commence in 2003 and last for 3 years. During this time there will be neither pedestrian nor vehicle access across the railway lines and canal at this point. The high level access to Paddington Station will also be closed.

As a result, it is predicted that a major focus of pedestrian movements between Paddington Station and the area to the north will be via the canalside, crossing the canal at one of the two new bridges; either at PaddingtonCentral, linking to the Porteus Road subway, or the new canal bridge at Monsoon. We are aware of the City Council's proposals to upgrade the Porteus Road subway and suggest that this project is completed before the commencement of the LTVA works.

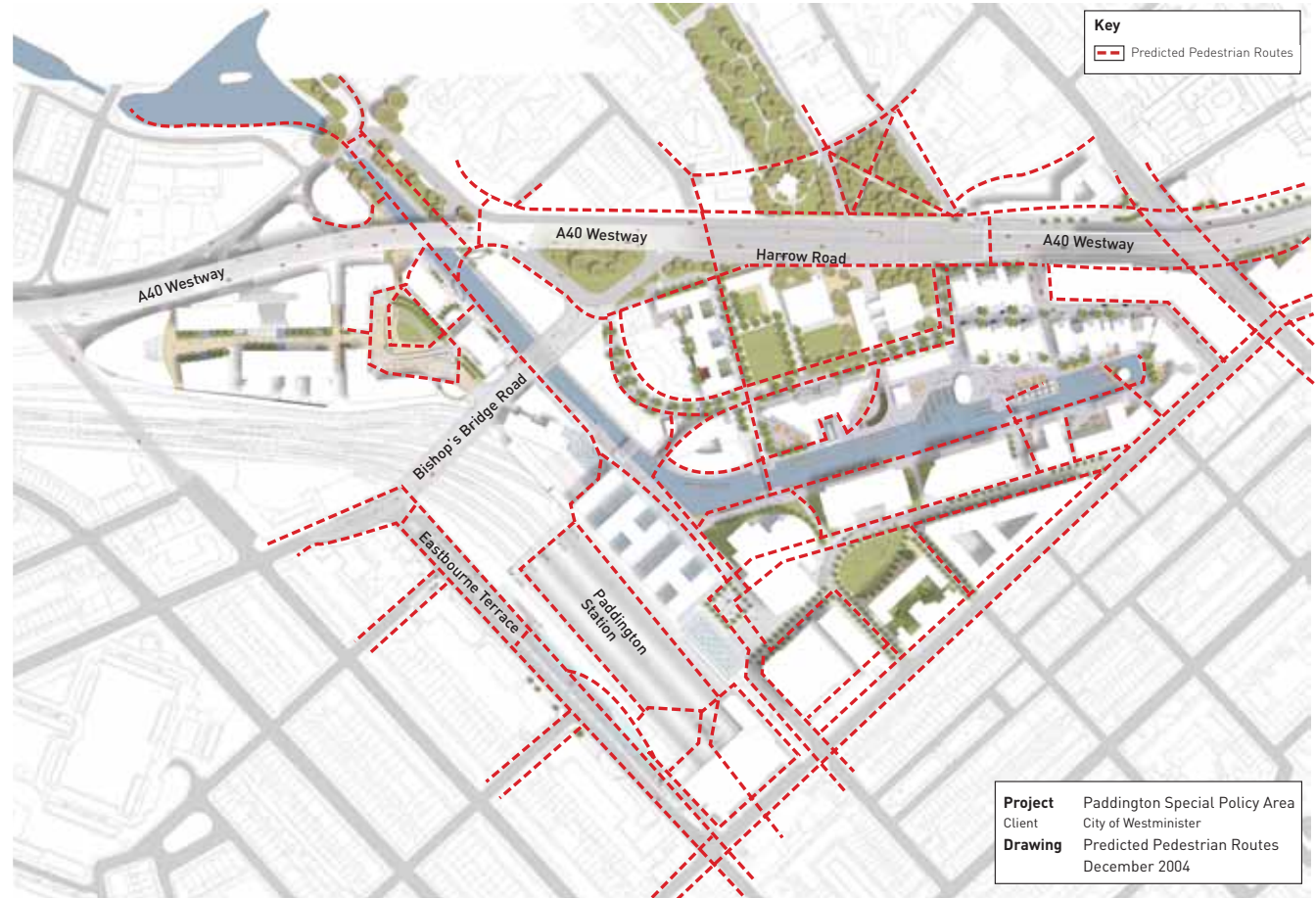
Other development proposals, including the Hermitage Street Site and PaddingtonCentral Phases 2 and 3, will be under construction in 2003, but will not change established pedestrian movements.



9.0 Pedestrian Routes

By the end of 2004: the canalside route beneath Bishop's Bridge will remain open, but its exact alignment may vary due to a phased sequence of works.

When the Grand Union Building and the Windings site are under construction the eastern part of the northern site of the basin will be affected and will need to be closed for safety reasons. The subway under the Marylebone flyover will be closed.



9.0 Pedestrian Routes

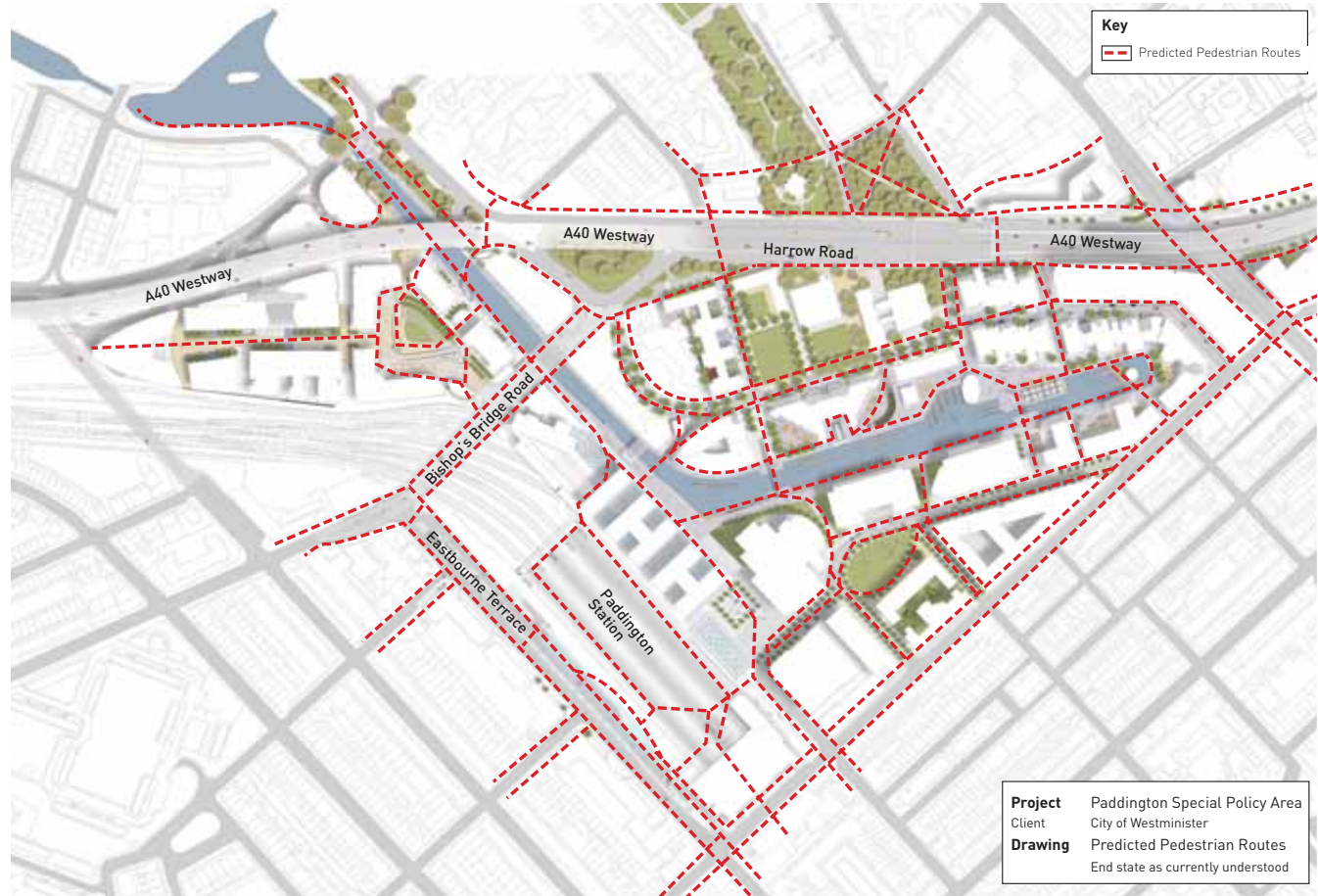
End State as currently understood: it is predicted that the LTVA works will be complete and Bishop's Bridge will reopen to traffic and pedestrians. However, the high level walk between Bishop's Bridge and Paddington Station will not be replaced. Access from Paddington Central to Bishop's Bridge will be re-established, allowing greater ease of north-south movements through this development.

At the St Mary's Hospital development a boardwalk along the basin will permit pedestrians to walk around the perimeter.

Factors affecting pedestrian movements along Eastbourne Terrace include the refurbishment or redevelopment of the buildings on the south-west side which are programmed between 2005 and 2008.

The other major impact of development within the area is the construction of CrossRail and its use of Eastbourne Terrace as a worksite. This proposal would significantly reduce the ease of pedestrian movements along this road. At the time of writing, the timetable for CrossRail is uncertain, but is provisionally shown as affecting Eastbourne Terrace for a number of years.

The development of Paddington Station, including the reconstruction of Span 4 and major alterations to the vehicle circulation, is being considered. Whilst no programme is published, it is accepted by Network Rail that this work cannot proceed whilst the construction of CrossRail is in progress.



9.0 Pedestrian Routes

To summarise: the effect of all these development proposals is to provide significant improvements to pedestrian access across the area, with the introduction of new canal walks and crossings. However, the limiting factor remains the A40 Harrow Road structure and its penetration by only two subways and the canalside walk on the length between Edgware Road and Westbourne Bridge.

It is understood that discussions have taken place between TfL Street Management and Westminster regarding the Edgware Road subway and that both would like to see the subway removed since there are now adequate surface level crossings. This would leave the subway available for other uses.

10.0 Cycling Routes

Cycling within Paddington Waterside is encouraged. In principle cycle routes should be located one block back from the canalside route. However, with various uncertainties regarding the developments within Paddington Waterside it may take some time to achieve satisfactory long term routes.

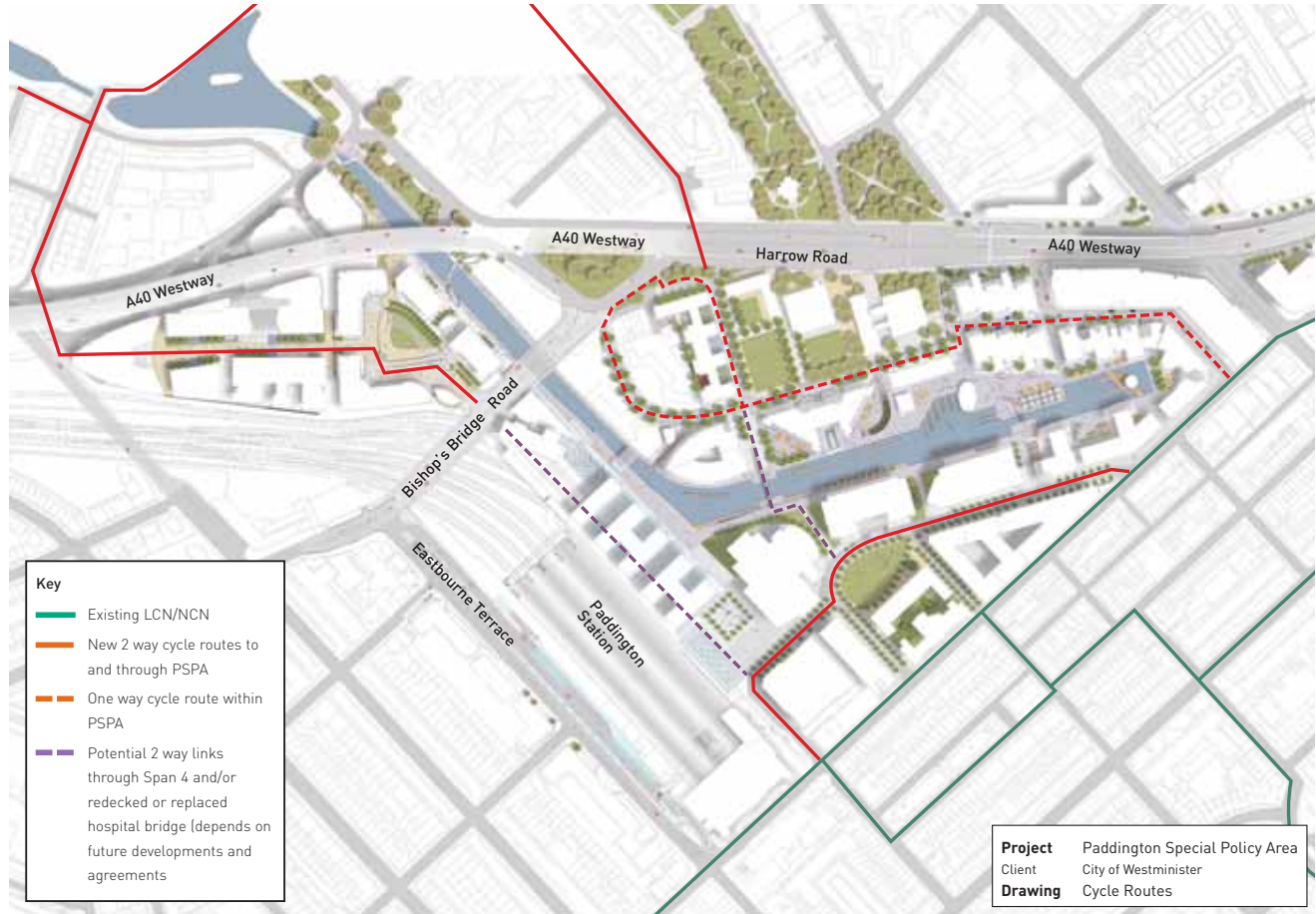
Therefore medium term routes on both the north and south side of the canal are proposed and shown in the plan opposite.

The north side route utilises the crossing facility on Harrow Road (between St Mary's Square and Hermitage Road) which could be modified to accommodate cyclists as well as pedestrians.

To the west of the canal the route will run via Westbourne Terrace Road and will reconnect with Bishops Bridge Road after reconstruction.

No cycle route will run along the canalside.

The two missing links in the network at the time of writing are the St Mary's Hospital Bridge which is to be reconstructed in the long term to accommodate both cyclists and pedestrians and the linkage through the station which will be provided via lifts that can accommodate cycles.



11.0 Public and Private Lavatory Provision

The new quarter of Paddington Waterside will need to be served by toilets available to the public so that it remains a clean, pleasant and odour free place in which to live, work and visit.

The existing main line station has some provision as do the public gardens at the northern end of the Special Policy Area. These are known as Rembrandt Gardens.

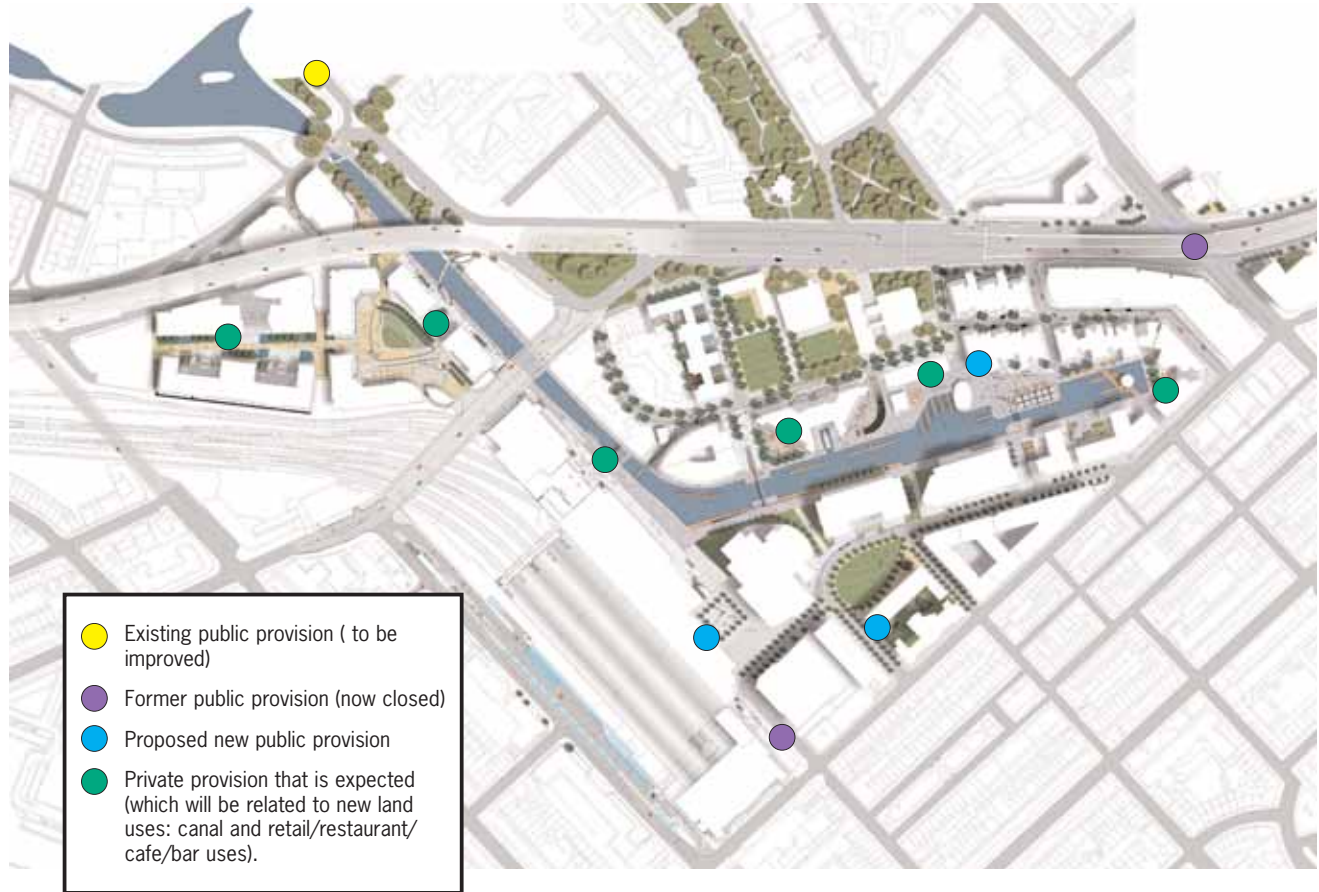
Public facilities in London Street and the provision in the Edgware Road subway has been closed due to abuse. The existing and former provision is shown on the adjacent map together with proposed locations.

The Cafes and restaurants will, of course, have their own facilities to serve patrons, but to avoid the problem of soiled streets and public areas, it is suggested that the Rembrandt Gardens lavatories be improved.

The new interchange at Paddington Station will include new lavatories on the concourse.

Canal users will have keyholder access with their mooring arrangements to use lavatories in the Facilities Building at the western side of the canal arm landing to the Basin.

The redevelopment of St Mary's Hospital and the Grand Union Building should include a lavatory available to the public, but it is not proposed to replace the facilities at Edgware Road.



Paddington Special Policy Area (Paddington Waterside)

Appendix—Product Reference Sheets

Public Realm in Private Developments

1	Paving	PAn.01
2	Corduroy Paving	PAt.03
3	Seating	SEs.02
4	Litter Bins	BLc.01
5	Bollards	BOm.02
6	Cycle Stands	CYm.02
7	Railings	RAp.02
8	Lighting	LLs.02, LLs.04, LLu.01
9	Tree Surrounds	TGp.02
10	CCTV	CTp.01
11	Signing	SLp.02

Public Highway

1	Paving	PAc.03
2	Granite Kerb	KEEn.01
3	Lighting	LLs.03, LLs.04
4	CCTV	CTp.01
5	Tree Surrounds	TGp.02

Canalside

1	Paving	PAn.01
2	Lighting	LLs.02, LLs.04
2	Signing	SLp.02
3	Tree Surrounds	TGp.02
4	Railings	RAp.02

Item:

Slab Paving

Description:

Natural granite stone paving

Specification:**Material**

natural grey granite, exact colours and description varies to suit surroundings.

Size

750x600mm or 600x600mm, 75mm thick

Finish

all sides same, top surface flame-textured or tooled to fine-picked finish

Options

granite-tooled setts 100x100x100mm thick, sawn, fine-picked top surface

Supplier:

Pisani Ltd.

Contact:12 Transport Avenue
Great West Road
Brentford
Middlesex TW8 9HF
Tel 01629 823481, Steve Corbett**Sheet Prepared:**

27-06-02



Item:
Pre-Cast Concrete Corduroy Paving
Description:
400x400x50mm pre-cast concrete paving

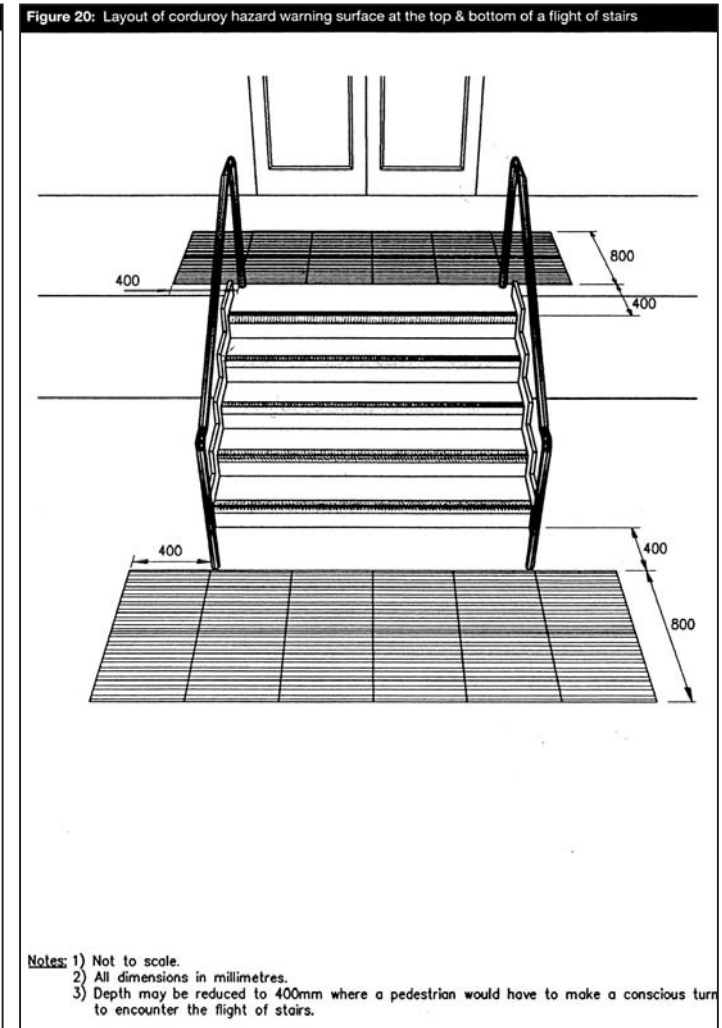
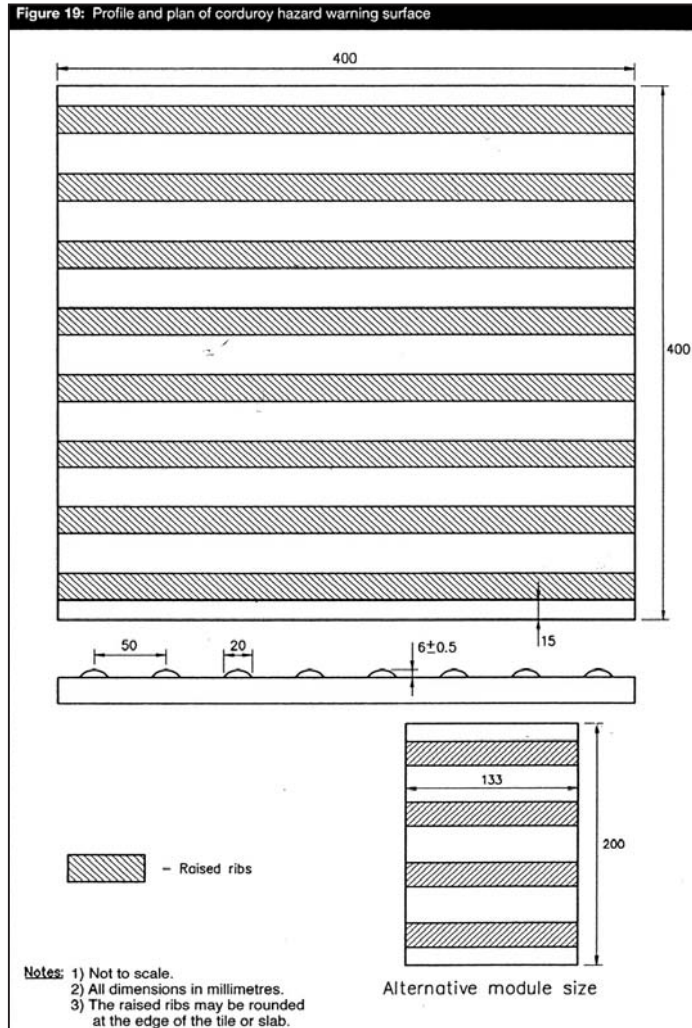
Specification:
Buff coloured paving to be used to top and bottom of steps within the public areas of private developments

Paving to be laid over 1:3 sand:cement mortar on 150mm grade C20 concrete base.

Supplier:
Marshall Mono Ltd.

Contact:
Sunderland Road
Sandy
Bedfordshire SG19 1QY
Tel 01422 306000

Sheet Prepared:
13-05-03



Item:

Bench

Description:

Geo Bench System

Specification:

Seat

Iroko hardwood, 316 grade stainless steel, galvanised mild steel. Natural finish timber, shot peened stainless steel, M10 painted mild steel. Flange plate fixing to foundation pads 150mm below finished level. Alternative finishes (sealed hardwood, satin stainless steel), painted legs, planted root fixing.

Ground anchors:

Ground fixing by metal anchor bolt below 180x180x180mm C20P in situ pad cast under paving surface.

Dimensions:

Length 1800mm, width of seat 525mm, width of bench 675mm, height of seat 790mm, height of bench 440mm

Product Codes:

bench: G-SB 11100
 seat: G-SB 21100
 seat with arms: G-SB 21100 AR

UK reg des no:

bench: 2091950
 seat: 2091951

Supplier:

The Woodhouse Co Ltd.

Contact:

Richard Wignall
 Spar Park
 Leamington Spa
 CV31 3HL
 Tel 01926 314 313

Sheet Prepared:

27-06-02



Item:

Litter Bin

Description:

Geo litter bin

Specification:**Height**

Overall height up to post top 1150mm

Diameter

bin body 500mm

Material

Stainless-steel 316 grade. Optional mild steel post, with stainless steel cap

Capacity

70 litres

Finish

Satin (240 grit brushed)

Fixing

Separate planted root, 150mm depth flange plate or surface fixed

Product

	Single	Twin	Triple
--	--------	------	--------

Stainless post:	G-LB 21100	G-LB 21110	G-LB 21120
-----------------	------------	------------	------------

Steel post:	G-LB 31100	G-LB 31110	G-LB 31120
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Options

Attachment to Geo lighting columns, galvanised steel body and support post to minimise street clutter, it is recommended that litter bins are attached to lighting columns wherever possible.

UK reg des no

2089412

Supplier:

The Woodhouse Co Ltd.

Contact:

Richard Wignall
Spa Park
Leamington Spa
CV31 3HL
Tel 01926 314 313

Sheet Prepared:

24-07-02



Item:

Bollard

Description:

Bollards by The Woodhouse Co Ltd. Geo System

Specification:

Dimensions

diameter 100/140/204mm, height 1100mm above ground
200mm below ground.

Material

bead blasted stainless steel Grade 316 body
brushed stainless steel Grade 316 top cap

- 100 codes : standard
- : G-B0 11100
- 140 codes : standard
- : G-B0 11110
- 204 codes : standard
- : G-B0 11120
- options : removable bollard with twist lock or lift
 : out ground socket galvanised body with or
 : without paint finish integral 450mm root
 : (separate on 204mm). These bollards can be
 : supplied as fixed or demountable.

Supplier:

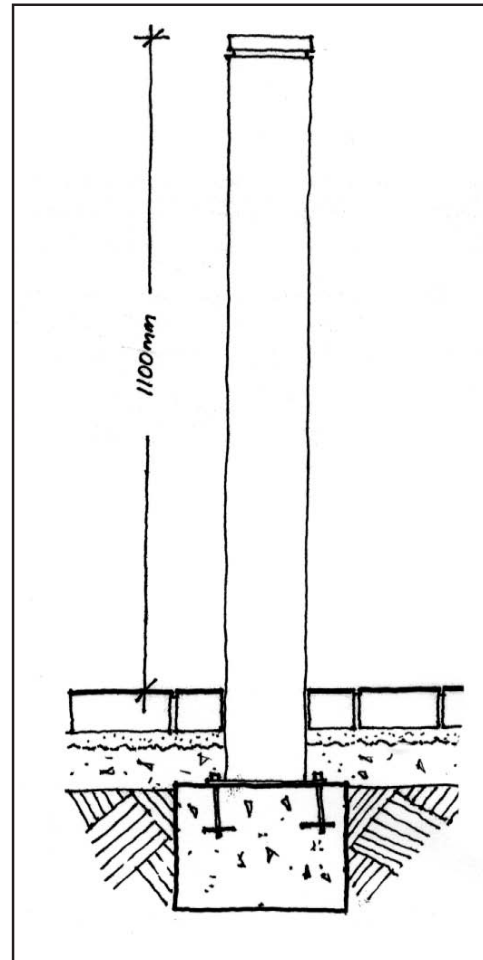
The Woodhouse Co Ltd.

Contact:

Richard Wignall
Spa Park
Leamington Spa
CV31 3HL
Tel 01926 314 313

Sheet Prepared:

24-07-02



Item:
Cycle Rack

Description:
Geo 2 full loop bike rack, modified to allow cycle parking sign to be installed.

Width
640mm

Height
850mm above the ground
400mm below the ground

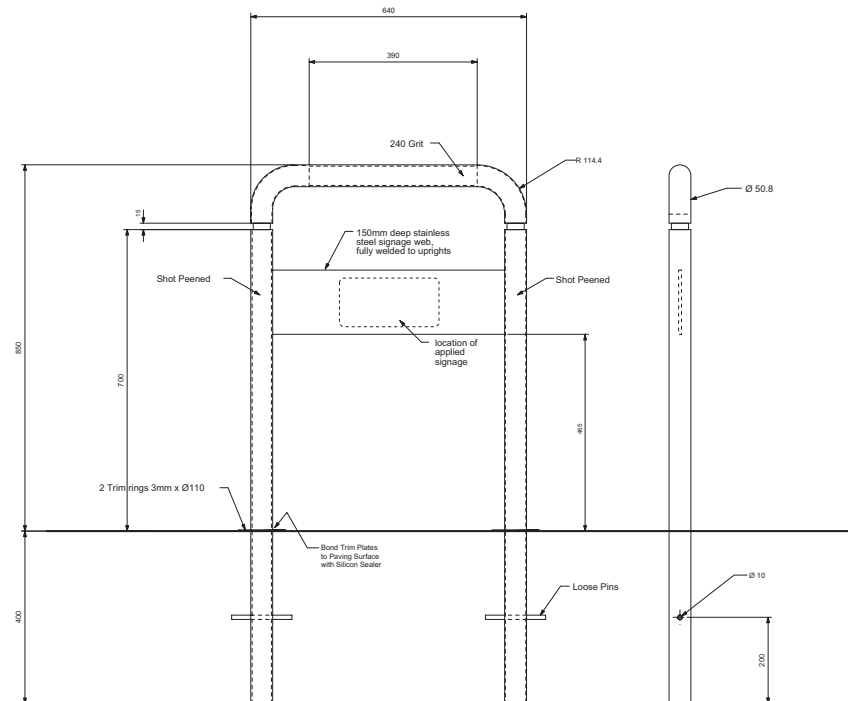
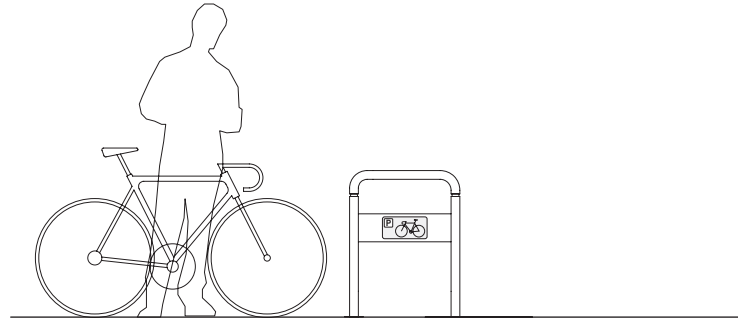
Materials
stainless steel 316

Finish
240 Grit

Supplier:
The Woodhouse Co Ltd. Geo System

Contact:
Richard Wignall
Spa Park
Leamington Spa
CV31 3HL
Tel 01926 314 313

Sheet Prepared:
27-07-02



Note :
Material : Stainless steel 316



Item:

Pedestrian Guard Railing

Description:

Stainless steel pedestrian guard rail, used to prevent pedestrians from straying into the carriageway or canal

Specification:**Materials**

Stainless steel

Finish

Stainless steel polished, bead blasted and satin (240 grit brushed)

Fixing

Site specific, but normally by means of flange plates fixed beneath finished level

Options

Alternative finishes, galvanised steel posts and panels painted to highway authority specification, information or decorative panels, integral fibre optic light points, fluorescent fixtures underneath the handrail.

Supplier:

The Woodhouse Co Ltd.

Contact:

Richard Wignall
Spar Park
Leamington Spa
CV31 3HL
Tel 01926 314 313

Sheet Prepared:

10-06-03



Item:

Lighting unit for non-adopted spaces

Description:

Geo Park Light unit (Flat or Curred Wing option)

Specification:

Mounting Heights

4 to 5 metres

Body

cast aluminium

Finish

Powder coated silver RAL 9006, underside of reflector white

Fixation

Pole top

Protection

IP65, lamp and gear compartment

Lamps

CDM-T 150 watt

Options

4200k (cool white), alternative finishes, integrated photo-electric cell, reflector with perforations. Control gear and isolator to be installed in buried pit adjacent to column; there is no provision to house control gear within the column.

Product Codes

asymmetric: G-LA 32120

symmetric: G-LA 33120

Supplier:

The Woodhouse Co Ltd.

Contact:

Richard Wignall

Spa Park

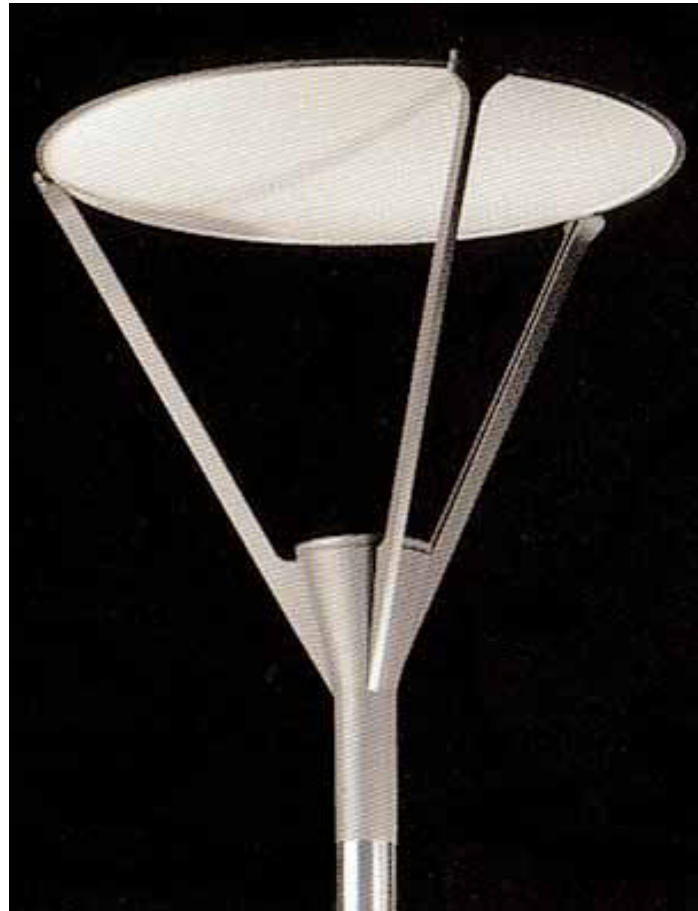
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Sheet Prepared:

24-07-02



Flat option



Curred Wing option

Item:

Lighting unit for minor roads and non-adopted spaces

Description:

Geo Direct Westminster Lantern

Specification:**Mounting Heights**

4 to 5 metres

Body

cast aluminium

Finish

Powder coated silver RAL 9006 or black (public highway)

Fixation

Pole top

Protection

IP65, lamp and gear compartment

Lamps

CDM-T 150 watt

Options

4200k (cool white), alternative finishes, integrated photo-electric cell, reflector with perforations. Control gear and isolator to be installed in buried pit adjacent to column; there is no provision to house control gear within the column.

Product Codes

asymmetric: G-LF0255-10A

symmetric: G-LF0255-10B

Supplier:

The Woodhouse Co Ltd.

Contact:

Richard Wignall

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Sheet Prepared:

11-03-03



Item:
Flush-Mounted Uplighter

Description:
Platek Mammouth Medio inground, recessed uplighter mounted flush with top of paving surface. Stainless-steel trim.

Specification:
Platek Mammouth Medio range metal halide uplighter fittings with vandal-resistant double glass cover at 253mm diameter and 382mm deep-die-cast aluminium, submersible to 10m, IP68, condensation-free. Fittings to be laid over drainage layer to manufacturers specification.

- Range:**
- 80.30.086 35W CDM-R 10-degree beam
 - 80.30.081 35W CDM-T 40-degree beam
 - 80.30.120 70W CDM-R 10-degree beam
 - 80.30.111 70W DDM-T 40-degree beam

Supplier:
Woodhouse UK Plc.

Contact:
Richard Wignall
Spa Park
Leamington Spa CY31 3HL
Tel 07977 240 292

Sheet Prepared:
02-07-02

DISCHARGE LAMP: HALIDE-SODIUM-MERCURY					
CODE	DESCRIPTION	POWER	POWER UNIT	LIGHTING ANGLE	DRAWING REFERENCE
8030079	CDM-TC SOCKET G8.5	35W	METAL HALIDE		M Model
8030080	POWERSTAR HQI-T SOCKET G12	35W	METAL HALIDE	6°	M Model
8030081	POWERSTAR HQI-T SOCKET G12	35W	METAL HALIDE	40°	M Model
8030086	PAR 20 CDM-R SOCKET E27	35W	METAL HALIDE	10°	M Model
8030120	PAR 30 CDM-R SOCKET E27	70W	METAL HALIDE	10°	MM Model
8030116	POWERSTAR HQI-TS SOCKET R7.5	70W	METAL HALIDE	50°	M Model
8030111	POWERSTAR HQI-T SOCKET G12	70W	METAL HALIDE	40°	M Model
8030109	CDM-TC SOCKET G8.5	70W	METAL HALIDE		M Model
8030110	POWERSTAR HQI-T SOCKET G12	70W	METAL HALIDE	6°	M Model
8030130	VIALOX NAV-E SOCKET E27	50W	SODIUM VAPOUR	84°	L Model
8030134	VIALOX NAV-TS SOCKET R7.5	70W	SODIUM VAPOUR	70°	M Model
8030104	DE LUXE HQI-R SOCKET E27	80W	MERCURY VAPOUR	86°	L Model
8030100	DE LUXE HQI-E SOCKET E27	50W	MERCURY VAPOUR	70°	L Model

• **OSCILLATING OPTICS**
▶ **TO BE PREPARED**



Double Glass Sleeves

H=317	Code 8925001
H=382	Code 8925002
H=468	Code 8925003

DIMENSION	
TYPE	H
S model	317
M model	382
L model	468

DOUBLE GLASS

PLATEK LIGHT has developed DOUBLE GLASS in order to reduce external glass temperature for safety problems of products mounted at ground or floor level, in areas with pedestrian or animal walkways. It maintains the maximum temperature under the one requested by the standard on the glass.

Resistant up to 1,500 kg loads. Driveover. Made up of an aluminium sleeve to fix tempered glass with gasket and two cast aluminium rings, flush mounted at ground level to block double glass.

Item:

Tree Pit

Description:

Tree Surround separated from surrounding granite paving by L-shape black mild steel profile (100x50x10). Neatly trimmed with Resin Bound Aggregate (size 10mm) and watering tube.

Size

Ideal 1.2m x 1.2m or min 1x1m

Process

Resin Bound Surface course consisting of aggregate particles, fully coated with two-part chemically curing, uv stable, flexible, crystal clear resin, hand finished by trowel and roller.

Colour

Granite Grey or various other

Supplier:

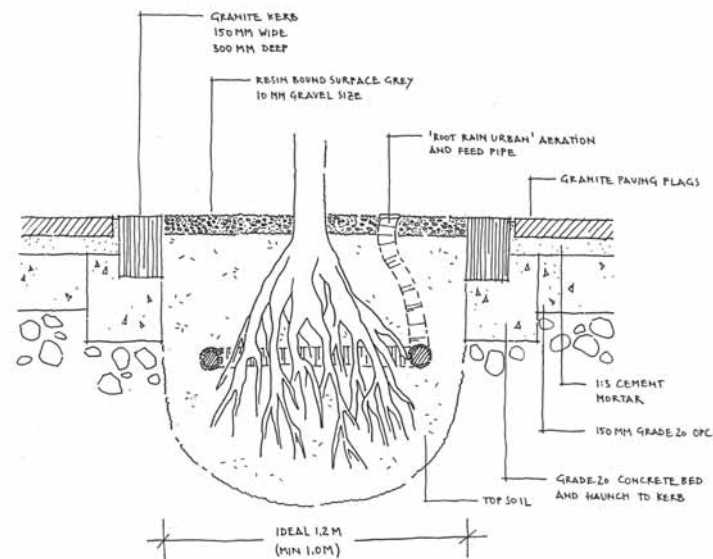
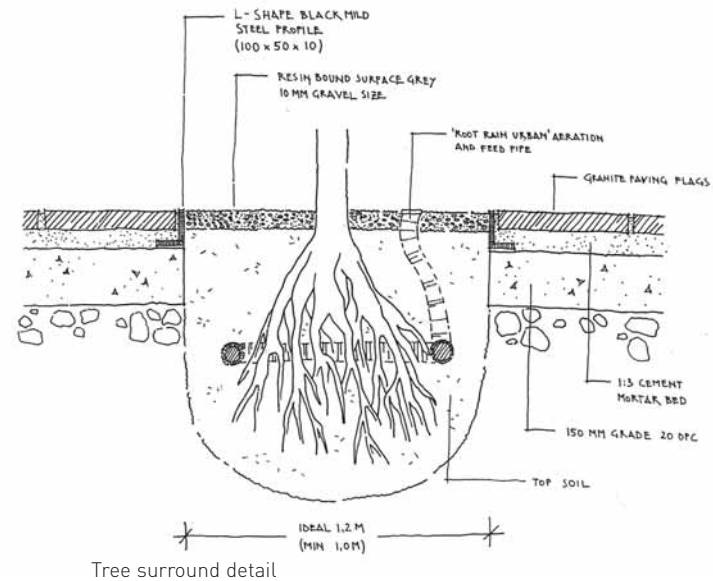
SureSet UK Ltd

Contact:

Resin Bond Aggregate
Unit 32, Deverill Road Trading Estate
Sutton Veny
Warminster BA12 7BZ
Tel 01985 84 11 80
www.sureset.co.uk

Sheet Prepared:

10-03-03



Various colours and aggregate sizes available



Item:

Pole for CCTV camera

Description:

Geo Light column

Specification:**Mounting Heights**

4 to 5 metres

Body

cast aluminium, tapering

Finish

Powder coated silver RAL 9006

Fixation

Pole top

Options

alternative finishes in mild steel or stainless steel.

Supplier:

The Woodhouse Co Ltd.

Contact:

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10-03-03



Item:
Pedestrian Signage System

Description:
Geo pedestrian signage system

Specification:

Post
Diameter 90mm height 3.2m

Finger
Dimensions 800x100mm

Material
316 grade stainless steel post, extruded aluminium fingers

Finish
240 grit brushed (satin) steel post, polyester powder coated fingers, lettering is engraved and backfilled.

Font - white Akzidenz Grotesk.
Background colour blue RAL 5002 matt.
Lower case character height 27.5mm.

Fixing
flange plate or separate buried base

Options
Attachment to Geo lighting columns, map-based signs, edge illumination, coloured background or lettering to fingers

Supplier:
The Woodhouse Co Ltd.

Contact:
Richard Wignall
Spar Park
Leamington Spa
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Tel 01926 314 313

Sheet Prepared:
02-07-02



Item:
Flag Paving for Paddington

Description:
Reinforced concrete paving flag with exposed granite aggregate finish

Specification:

Material
concrete, reinforced mesh

Aggregate
natural granite mix known as the "Paddington"

Sizes
600x600mm
600x750mm
600x450mm

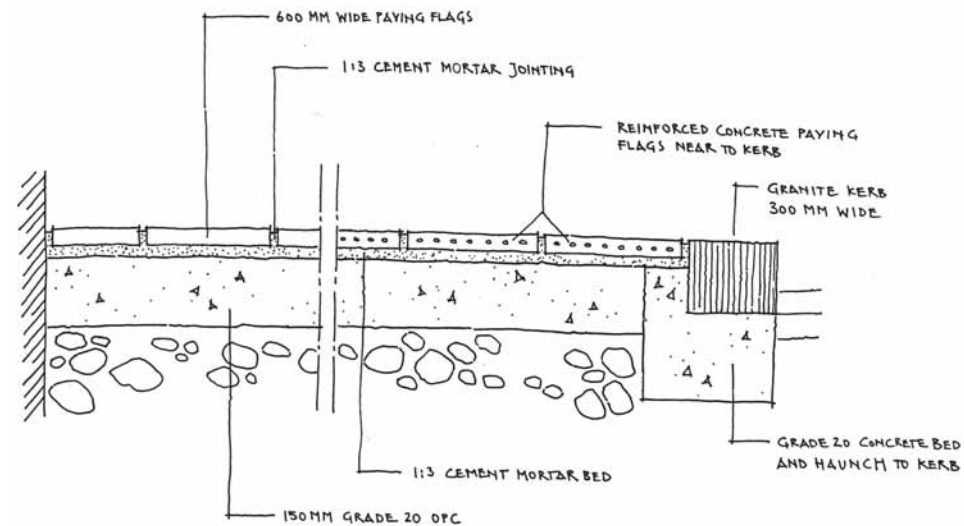
Thickness
63mm

Finish
exposed aggregate

Supplier:
Charcon Hard Landscaping

Contact:
Peter Brown
Hulland Ward
Ashbourne
Derbyshire DE6 3ET
Tel 01335 372222

Sheet Prepared:
27-06-02



Item:

Major Road Lighting Unit

Description:

Geo Street Lighting Fixture, 6m column, tapered, reference GLF0255-08

Specification:

6m Cylindro Conical Column, 1000mm Integral Buried Root, 168mm Base Diameter, Flush Fitting Access Door, Hot Dipped Galvanised, Painted with MIO Base Coat plus finished jet black RAL9005.

G-BA 03300 Top Coat, Complete with Backboard for Fused Isolator.

Bracket

800mm Mild Steel Outreach Bracket, finished jet black to RAL9005. G-BA 03300

Luminaire

Geo disc 600 diameter, cast aluminium, and finished in powder coat to RAL 9007. G-LA 22***

Lamps

150 watt CDM-t Lamp

Protection

IP66

Supplier:

The Woodhouse Co Ltd.

Contact:

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Sheet Prepared:

27-07-02

